

Key Strategies

No single activity will achieve the vision of the community. Recommendations from several, perhaps all, of the subcommittees, when combined, result in a strategy of multiple actions or activities that will achieve that vision. Each Key Strategy consists of activities for a single complex project or theme that the neighborhood considers critical to achieving its vision for the future. Some of the recommendations require City action. Others are recommended community activities. While the Key Strategies are high priorities for the neighborhood, they are also part of a twenty-year plan, so the specific activities within each Key Strategy may be implemented over the span of many years.

There are three Key Strategies in the Denny Regrade Neighborhood Plan:

- Green Streets and Open Space Connections Strategy
- Strategy to Sustain Belltown's Character
- Strategy to Sustain Adequate Parking

Each of the Key Strategies is described below, together with the recommendations that make up that strategy. Each of these recommendations will appear later in this document in the respective section from which it originated, along with other recommendations that stand alone.

A Green Street is defined as a street right-of-way which is part of the street circulation pattern, and through a variety of treatments, such as sidewalk widening, landscaping, traffic calming, and pedestrian-oriented features, is enhanced for pedestrian circulation and open space use.

The function of Green Streets is primarily to (1) enhance public circulation and open space opportunities, particularly in medium to high density areas lacking open space; (2) reinforce the pedestrian environment of neighborhood commercial areas; and (3) develop a street network that includes neighborhood amenities such as pedestrian and bicycle trails connecting open spaces with activity areas

Green Streets and Open Space Connections Strategy

Description: The Green Streets and Open Space Connections Strategy combines a series of actions that will provide parks and open space opportunities for residents of the Denny Regrade without a significant expenditure of public funds for land acquisition. The strategy seeks to improve Green Streets within the community and to improve and enhance connections to Open Spaces outside, but near, the neighborhood, most notably the Myrtle Edwards Park and the Seattle Center.

KEY RECOMMENDATIONS

PEDESTRIAN ENVIRONMENT ~ GREEN STREETS AND OPEN SPACE

Recommendations:

- ♦ Preserve and expand the Belltown P-Patch through the acquisition of additional property, including the cannery cottages and the "inset" lot within the P-Patch.

- ◆ Provide more green space in the Regrade neighborhood through implementation of Growing Vine Street and other Green Streets, as their designs are developed.
- ◆ Designate Clay Street, Eagle Street and Bay Street as Green Streets. Extend the Vine Street Green Street designation to Denny Way.

- ◆ Develop a process within the structure of City government based upon the DUCPG Green Streets, Open Space and Urban Design recommendations, wherein the procedures and responsibilities for implementing and maintaining Green Streets is clear and concise.



Vine Street is one of Belltown's designated Green Streets that provides access to the waterfront.

- ◆ Implement the Bell Street improvements as planned by the Denny Regrade Business Association (DRBA).
- ◆ Improve alleys as connections to and through Green Streets, by such methods as creating mid-block crossing and traffic bulbs, screening dumpsters, installing lighting, promoting alley entrances, naming alleys and retaining brick paving. Alley connections across arterial streets should not be considered.
- ◆ Retain existing at-grade crossings of the railroad tracks at Wall Street, Vine Street, Clay Street, Broad Street and Bay Street. Develop rest/information areas adjacent to railroad crossings that are designed to draw people into Belltown.
- ◆ Require a pedestrian connection to the waterfront through any future development of the vacant lots (Unocal site) on the western

end of Eagle Street. Recommend a private pedestrian overpass be installed over the railroad tracks to complete the connection from the Seattle Center to the waterfront and Myrtle Edwards Park.

- ◆ Improve pedestrian connections between the Regrade and the Seattle Center by connecting with the KOMO TV improvements at 4th and 5th Avenues and Denny Way, and connecting to the waterfront to the Seattle Center via Eagle Street Green Street as a continuation of Potlatch Trail (see Queen Anne Neighborhood Plan).

LAND USE

Recommendations:

- ◆ Ensure thorough design review that development in the area north of Broad Street encourages and accommodates pedestrian movement between the Seattle Center and the Waterfront/Myrtle Edwards Park, and into the Belltown Neighborhood. New development shall facilitate pedestrian and ADA access from 1st Avenue to the waterfront.

TRANSPORTATION

Recommendations:

- ◆ Oppose the Broad Street overpass project since the benefit to traffic congestion and delay does not outweigh the impact to the neighborhood. Support developing a plan for and implementing changes to de-emphasize the use of Alaskan Way by through traffic between the Ferry Terminal and Pier 70 as part of the Alaskan Way Harborfront Public Improvement Plan.
- ◆ Improve pedestrian crossings. Intersections with the highest priority for improvement include: 3rd Avenue/Denny Way, 2nd Avenue/Denny Way/Eagle Street, 1st Avenue/Denny Way, Vine Street/Elliott Avenue, Vine Street/Western Avenue, Vine Street and 3rd Avenue, 3rd Avenue and Cedar Street, Bell Street/Western Avenue, Lenora Street/Elliott Avenue, and Broad Street/Alaskan Way. Potential improvements should include crosswalks, lighting, signage and pedestrian signalization.

Strategy to Sustain Belltown's Character

Description: The Strategy to Sustain Belltown's Character is a combination of activities and recommendations intended to ensure that, in the future, when the neighborhood becomes the densest residential community in the city, the Belltown Neighborhood is truly a mixed use, mixed income, eclectic community that provides the broad range of services required for a vibrant and healthy downtown residential community. This strategy is also intended to guide growth in a manner that maintains the physical character of the neighborhood.

KEY RECOMMENDATIONS

COMMUNITY ENRICHMENT AND SOCIAL SERVICES

Recommendations:

- ◆ Officially change the name of the Denny Regrade Urban Center Village to the Belltown Urban Center Village.
- ◆ Establish Belltown Multi-Purpose Neighborhood Center providing recreational opportunities, youth activities, senior services, childcare and adult day-care, meeting rooms and space for public and private events. Provide for commercial space as a method of self-sustaining funding for the community center and as a method of maintaining and promoting the eclectic nature of Belltown small businesses.
- ◆ Develop a community school and encourage community educational programs and partnerships with area business and crafts people utilizing the resources of Antioch University, the Pacific Science Center, Children's Theatre, the Seattle Arts Institute, non-profits, social service organizations and other groups and organizations.
- ◆ Work with Denny Triangle Neighborhood to secure a major grocery store.

PUBLIC SAFETY AND NEIGHBORLY REGULATIONS

Recommendations:

- ◆ Work with Seattle City Light to install pedestrian oriented lighting on the streets, with 3rd Avenue being the highest priority.
- ◆ Continue and expand Denny Regrade Action Team (DRAT) walks through the neighborhood. Implement Block Watch programs.

HOUSING AND LAND USE

Recommendations:

- ◆ Create a Denny Regrade Housing Committee as a subcommittee of the neighborhood organization that follows up on the neighborhood plan implementation.
- ◆ Encourage the development of additional resources for the preservation of existing affordable housing. Consider including housing as a use of funds from the neighborhood bond issue.
- ◆ Recommend that the City develop a land-banking program and focus on the mid-block sites in the Denny Regrade to control sites before land costs increase further.
- ◆ Make live/work space an eligible street level use where street level uses are required by the land use code.
- ◆ Designate Denny Regrade as a Conservation District and develop design guidelines that will describe and preserve the architectural character of the neighborhood.

- ◆ DCLU should undertake a study to expand the area where Street Level Uses (retail and service uses) are required. Currently, Street Level Uses are required on 1st and 3rd Avenues from Cedar Street to Stewart Street, and on portions Cedar Street, 4th and 5th Avenues in the Five Corners area.

Alternative 1: Expand the area to include Western Avenue, 2nd Avenue, 4th Avenue and 5th Avenue. Extend the area from Cedar Street to Denny Way.

Alternative 2: Expand the area to include 2nd Avenue between Cedar Street and Virginia Street and all of the east-west streets between 1st Avenue and 3rd Avenue, with a recommendation that design review encourage live/work space on the streets, especially near the alleys.

- ◆ Ensure, through design review, that street frontages where Street Level Uses are not required, be designed in a manner that enhances the pedestrian environment. This may be accomplished through setbacks and extensive landscaping, public art, or similar means. Parking within a parking structure shall not be visible from ground level except through entrances and exits.
- ◆ Work with Historic Seattle regarding the viability of protecting historic or icon buildings.
- ◆ Establish Conservation or Community Heritage District in the Denny Regrade.

TRANSPORTATION AND PARKING

- ◆ Create a well designed, well maintained, well managed pedestrian multi-modal hub in conjunction with the Neighborhood Center that could be served by transit, jitney, and taxi, as well as provide other amenities such as bicycle storage lockers, telephones, and neighborhood retail services that are open in the evening.
- ◆ When street and sidewalk improvement programs are implemented, design the improvements in accordance with the Denny Regrade Streetscape and Pedestrian Environment Guidebook.



Maintaining the physical character of the Denny Regrade neighborhood through design review is a high priority for the community.



Strategy to Sustain Adequate Parking

Description : The Strategy to Sustain Adequate Parking is a combination of activities and recommendations intended to ensure that the supply of parking is maintained at a level that is adequate to serve neighborhood residents, businesses and employers. Currently, development of principle use parking is not permitted in the Denny Regrade and development of accessory parking is limited. As a consequence, each time a new development occurs on a site currently used as surface parking, there is a net loss in the supply of parking spaces.

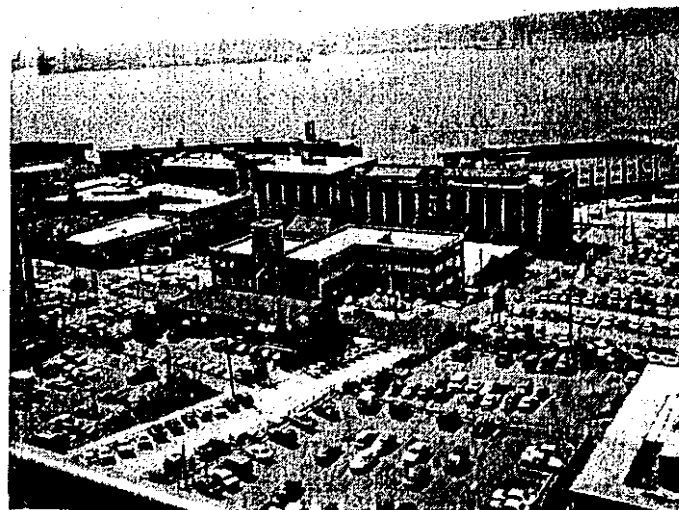
We do not want to become the parking lot at the end of the Metro Free-Ride area for Downtown Seattle— Park and ride lots are already designed for this. Off-street residential and employee parking is a must, in order that short and mid-term on-street parking is available for visitors to our shops and businesses. Zoning and construction rule changes are required to allow construction of new garage parking with new construction for residents of older buildings being preserved for historical purposes. Non-residential and non-business related long term parking should be discouraged.

KEY RECOMMENDATIONS

- ◆ Prepare an inventory of existing public parking spaces in the Denny Regrade. Establish a target number of parking spaces that should be sustained as the neighborhood develops
- ◆ Provide parking that is responsive to neighborhood needs (e.g., explore residential parking zones, extended meter hours, expanded private and public parking provisions in new construction, and evaluate parking script kiosks/meters).
- ◆ Support establishment of a “downtown parking fund”, as designated in the 1985 Downtown Plan, that would facilitate the construction of parking facilities. Expand the area for parking funds use to include the Denny Regrade.

There are about 60 surface parking lots in the Regrade, but they are rapidly being replaced by new development.

- ◆ Institute residential parking zones on streets with little commercial activity. Any RPZ designation should allow non-residents to park for a limited time.
- ◆ Allow public or private principle use parking structures in the Denny Regrade provided that the first floor be occupied by artists live/work housing or community uses. The ratio of area used for parking to area of other uses should be no greater than 2:1. Denny Regrade residents and businesses should be given first priority for use of parking.
- ◆ Allow developers to build more parking (for use by neighborhood residents and businesses) than is required by their development and expand the allowed distances between uses sharing parking.
- ◆ Increase the allowable distances between shared parking locations to greater than the existing 800-feet allowed. The City of Seattle should perform a study to determine the appropriate distance.
- ◆ Allow excess parking in a residential parking garage to be leased to residents who are not tenants of the building.
- ◆ Encourage the Denny Regrade Business Association (DRBA) to work with the operators of surface parking lots to limit the number of spaces that are available for all-day parking.





Each of the Subcommittees presented their recommendations to the community at a Neighborhood Picnic in Regrade Park

Plan Elements

The Key Strategies combine recommendations from several subcommittees. There are also many recommendations from each of the committees that are not included in the integrated strategies, but rather are intended for implementation by the City, community organizations and the Belltown community in both the near term and long term future.

The Denny Regrade Planning Committee organized into five subcommittees to develop the neighborhood plan. These committees include:

- Housing and Land Use
- Transportation
- Pedestrian Environment
- Community Enrichment and Social Services
- Public Safety and Neighborly Regulations

Although each of these committees met independently over the course of the planning period (the Transportation and Pedestrian Environment committees met jointly in the latter portion of the Phase II process), twice each month an overall Planning Committee meeting was held to share each committee's progress, concerns and recommendations.

The following sections present the recommendations of each of the subcommittees.